



# TULSA PRESERVATION COMMISSION

## STAFF REPORT

Thursday, May 12, 2016

Unified Design Guidelines – Revisions and Public Comments

---

### A. ITEMS FOR PUBLIC REVIEW

1. Revisions to the Unified Design Guidelines
2. Public Comments

### B. BACKGROUND

In January, the new Zoning Code brought a few changes to the Historic Preservation (HP) Overlay Districts that require the Unified Design Guidelines to be update. The Tulsa Preservation Commission held a public information session on Thursday, April 28, 2016 at Fletcher Hall, Christ the King Church. Residents and property owners were notified of the public hearing and through a neighborhood mail-out, the Tulsa Preservation Commission Blog, Interested Parties' E-Mail List, and Facebook. The drafts of the Unified Design Guidelines were made available through the Tulsa Preservation Commission Website, or a copy could be requested. If a resident and property owner could not attend the public information session, he or she could submit comments to Preservation Staff by telephone, e-mail, and mail or could attend the Tulsa Preservation Commission Regular Meeting on May 12, 2016 to deliver any comments or concerns before the Historic Preservation Committee reconvenes. The Tulsa Preservation Commission is still accepting public comment.

### C. ISSUES AND CONSIDERATIONS

1. Staff created a document of the comments from the public hearing held on Thursday, April 28, 2016 and comments submitted through e-mail. The comments are enclosed for the Commission review.

Good afternoon,

Since I will not be able to attend the Unified Design Guidelines Update meeting on Thursday, April 28<sup>th</sup>, I hope that you will consider these comments based on my review of the draft of the Unified Design Guidelines and my experience as a resident of the Brady Heights Historic District.

The first comment I have is a general one: although the Unified Design Guidelines allow for a one-stop-shop for how work should be assessed in Historic Preservation Overlay districts, some key provisions which originally existed in the guidelines for each individual neighborhood (I've linked to the original Brady Heights guidelines, still hosted on the TPC website) have been lost along the way and allowed for projects which would not have been allowed pre-2012 to go through under the Unified Design Guidelines. This comment will be returned to as I suggest guidelines specific to Brady Heights Historic District.

The second comment I have concerns **B.6.1** under Guidelines for Additions to Existing Structures and why “Detached buildings or structures, such as garages and sheds, not located in the street yard, as defined in the Zoning Code, are exempt from HP Permit review.” It is my opinion that any detached building or structure which is visible from the street should not be exempt from HP Permit review, as inappropriate design of such structures can be just as injurious to historic preservation efforts as any changes or work done on the primary structure on a lot. As a resident who may one day propose a new garage in my backyard, I would willingly submit to the HP Permit review process simply to ensure that anything I build on this corner lot—visible from N. Denver Ave. and E. Jasper St.—would be appropriately reviewed prior to approval for construction. Guideline **C.4.1** under Guidelines for New Construction would also be adjusted to reflect this change, in my preferred set of guidelines.

The third comment I have concerns **C.1** under General Requirements for Guidelines for New Construction and the establishment of a **C.1.6** guideline specific to **Brady Heights**. Our original individual guidelines stated, “New construction should be single family homes (one residence per standard lot) not multi-family residences including but not limited to apartments, condos, garden units, patio houses, townhouse, etc. or non-residential structures.” I don’t necessarily believe that a new guideline need be quite as restrictive as what was originally established, but I do believe that new construction in Brady Heights Historic District should reflect the RS-4 zoning applied to the majority of the neighborhood’s lots. This would permit limited multi-family residences like duplexes which can appropriately establish increased density while maintaining the appearance of single family homes which are a defining characteristic of the historic structures in the neighborhood; something that the construction of new apartments, condos, townhouses, etc. would have difficulty doing.

The fourth and final comment I have concerns **C.3** under Building Materials for Guidelines for New Construction and the establishment of a **C.3.1.1** guideline specific to **Brady Heights**. One of the most common complaints and sticking points about Historic Preservation Overlay in our neighborhood is that owners of contributing structures are required, per **A.4.5.1**, to match the original historic window material when replacement of a deteriorated window is necessary; meanwhile, new construction is permitted to use vinyl windows and other window materials which are inappropriate for a historic neighborhood and detract from the character of the district. A **C.3.1.1** guideline would state that original historic window material present in contributing structures for the district should be used in new construction, with no leeway for approval of vinyl windows.

If you have any questions or require any clarification of my comments, please feel free to contact me.

Thank you very much for your time and consideration,

Brian Parker  
Resident  
902 N. Denver Ave.  
Brady Heights Historic District  
Cell: (207) 577-8203  
Email: [brianjparker@outlook.com](mailto:brianjparker@outlook.com)

Good morning,

Of course, it occurred to me after sending off my comments last night that I'd forgotten to mention something, so I hope you'll indulge me with another email about the Unified Design Guidelines Update.

This comment doesn't fit nicely into a single added line to the guidelines, but I believe that the Historic Preservation Permit Process should have some special language added to suggest that Tulsa Preservation Commission will not review any project proposed if there is a Zoning Change or Special Exception being sought until that Zoning Change or Special Exception is granted or denied by the appropriate governing body.

For example, if somebody with a lot zoned RS-4 in a neighborhood with Historic Preservation Overlay protection wishes to change zoning to a different RS level, RM, etc., I would like to see a general guideline established where the TPC's Historic Preservation Permit process does not begin until after the lot has been approved for re-zoning (and thus the project can begin the HP Permit process) or denied (and thus a new design would be required for the lot).

This change would be in line with the language on page 5 of the Unified Design Guidelines which states, "Historic preservation zoning is technically referred to by city planners as 'overlay' zoning. The historic preservation zoning is 'overlaid' on top of the existing zoning. The existing zoning restrictions are unchanged – instead they are supplemented by the conditions of historic preservation zoning." This language can be interpreted to say that the HP Permit process should be begun after other zoning requests or requirements have been completed, as there are fundamental elements of the zoning code which are not the specific responsibility of the Tulsa Preservation Commission.

I wish I could say that this suggestion was simply theoretical, but in the last year there was a project requiring a Special Exception from the Board of Adjustment in Brady Heights Historic District which simultaneously went through the HP Permit process. Since the HP Permit process finished first and was approved, the developer proposing the project was able to state this approval to the Board of Adjustment and ended up being granted a Special Exception regarding lot size, setbacks, and use in a situation where one should not have been granted because there was no hardship involved, as per the legal requirements for such a Special Exception. With such a precedent on the record, I fear

that this loophole could be utilized in future cases where a developer could pit approvals of governing bodies against one another in order to obtain favorable changes to their properties which would not otherwise be granted.

As before, if you have any questions for me or require any clarification of my comments, please don't hesitate to contact me.

Thank you very much for your time and consideration, and your service to the Historic Preservation Overlay neighborhoods of Tulsa,

Brian Parker  
Resident  
902 N. Denver Ave.  
Brady Heights Historic District  
Cell: (207) 577-8203  
Email: [brianjparker@outlook.com](mailto:brianjparker@outlook.com)

Hello Mr. Porter,

On behalf of COHN members, I would like to offer a few concerns perhaps, suggestions to the draft recently offered by the TPC. COHN will not have time to meet before the scheduled meeting today and our members are unable to attend the TPC meetings, usually members email their comments to me, I have often asked them to forward all questions/concerns, hopefully some have complied.

I believe I can speak for most, and at a glance thru the guidelines today, I notice a few things I would like to bring to your attention.

**General Requirements** - regarding height of the finished floor elevation, "between the minimum & maximum finished floor elevation of those structures along the same side of the street." We would suggest BOTH sides of the street, which protect the overall district.

**Building Materials** - the use of "unfinished or clear-finished metals" which most die-hard preservationists will not be inclined to support. Though, it is 'new' as opposed to existing, the use of wood vs. metal will *always* be preferable.

**Building Materials** - "Deviation from the materials on existing historic structures in the district will be considered on a case-by-case basis".

Again, most preservationists do NOT believe any variance is necessary.

If someone wants to live in an existing historic neighborhood, the characteristics absolutely must be protected and adhered to, at ALL times, without exception, this is a slippery slope, in which we do not wish to engage.

There is a sentence regarding the **attached garages** which I found a bit problematic, though I think I understood the meaning; "Permitted attached garages shall be located so that the front façade of the garage is not located forward of the rear wall of the primary structure."

Most purists prefer detached garages period, any attached garages should always be in the rear or side of any new structure.

We appreciate the opportunity to review and are grateful for your tutelage over the guidelines, admittedly an important tool.

Thank you.

Cherie R. Cook

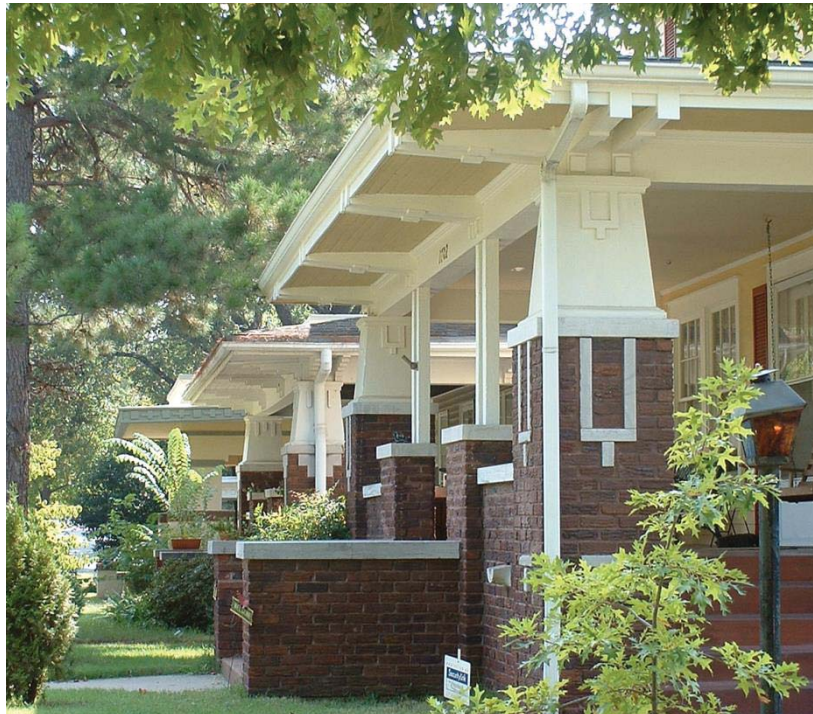
918.592.5492

#### Public Hearing Notes: Thursday, April 28, 2016

- There was a request for more clarification between floors and stories as they reference building height.
- There was request for why the Tulsa Preservation Commission is considering unfinished metals under the Guidelines for New Construction. Some examples given was unfinished copper is historically appropriate, and unfinished metals would be made on a case-by-case basis.
- Several comments were made regarding the review of the detached accessory structures and request the Tulsa Preservation Commission to review them. Detached accessory structures not located in the street yard are exempt from review.
- There was request for more clarification in the Guidelines for New Construction including selection of windows and doors and referencing "in the same district" in lieu of "along the same street" where a historic district shares a boundary line with another historic district.
- Discourage demolition of buildings for surface parking,
- There was concern for loss of historic interiors.
- There was concern for demolition of historic structures for surface parking.



# TULSA PRESERVATION COMMISSION



## **Unified Design Guidelines**

### **RESIDENTIAL STRUCTURES**

within

Historic Preservation Overlay Zoning Districts

ADOPTED: ((INSERT DATE))

## **UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES**

### **Tulsa Preservation Commission, 2016**

Robert Shears, Chairman  
Michael Craddock, Vice-Chairman  
David Schoell, Secretary  
David Pounds  
Tom Neal  
Susan McKee  
Peter Grant  
John Snyder  
Jim Turner  
Ted Reeds, II  
Michael Birkes

### **Tulsa Metropolitan Area Planning Commission, 2016**

Michael Covey, Chair  
John Dix, 1<sup>st</sup> Vice Chair  
John Shivel, 2<sup>nd</sup> Vice Chair  
Margaret Millikin, Secretary  
Ryon Stirling  
Gail Carnes  
Ted Reeds, II  
Joshua Walker  
Mike Fretz  
Michael Willis, ex-officio  
Dwain Midget, ex-officio

### **Tulsa City Council, 2016**

Jeannie Cue, Chair  
Anna America, Vice-Chair  
Jack Henderson  
David Patrick  
Blake Ewing  
Karen Gilbert  
Connie Dodson  
Phil Lakin, Jr.  
G.T. Bynum

### **Mayor of the City of Tulsa**

Dewey F. Bartlett, Jr.

*This publication was printed with Federal funds from the National Park Service, U.S. Department of the Interior. The contents and opinions do not necessarily reflect the views or policies of the Department of the Interior. The U.S. Department of the Interior prohibits discrimination on the basis of race, color, national origin,*



## UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES

disability or age. Any person who believes he or she has been discriminated against in should contact the Office of Equal Opportunity, National Park Service, 1849 C Street NW, Washington, D.C. 20240

### **TABLE OF CONTENTS**

<b>Historic Preservation Zoning Overview .....</b>	<b>5</b>
<b><u>Historic (HP) Preservation Permit</u> Process Overview .....</b>	<b>6</b>
<b>Section A – Guidelines for Rehabilitation of Existing Structures.....</b>	<b>9</b>
<b>Section B – Guidelines for Additions to Existing Structures.....</b>	<b>17</b>
<b>Section C – Guidelines for New Construction .....</b>	<b>25</b>
<b>Section D – Guidelines for Relocated Structures.....</b>	<b>31</b>
<b>Section E – Guidelines for Non-Contributing Structures .....</b>	<b>35</b>
<b>Section F – Guidelines for Demolition of Structures.....</b>	<b>37</b>
<b>Section G – Guidelines for Landscape Features, Paving and Signage.....</b>	<b>39</b>



## UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES

DRAFT

### **HISTORIC PRESERVATION ZONING OVERVIEW**

Historic neighborhoods seek out historic preservation (HP) zoning to provide extra protection from inappropriate alterations and unsympathetic new construction. Generally, the neighborhoods that would qualify for HP zoning are those that would be eligible for the National Register of Historic Places. For example, all of Tulsa’s HP-zoned districts are also National Register-listed districts. However, listing in the National Register does not require that the neighborhood pursue local HP zoning.

HP zoning requires that any exterior renovations or repairs (in some cases) are subject to design review by the Tulsa Preservation Commission before work can be done.

#### **Historic preservation zoning helps to:**

1. Preserve and protect the distinctive characteristics and the context of the district, protecting property owners’ investments.
2. Maintain and improve the streetscapes of those buildings and places.
3. Assure that new construction projects and additions to existing homes are architecturally compatible with existing buildings in the district.

Historic preservation zoning is technically referred to by city planners as “overlay” zoning. The historic preservation zoning is “overlaid” on top of the existing zoning. The existing zoning restrictions are unchanged – instead they are supplemented by the conditions of historic preservation zoning. The existing zoning requirements control such issues as land use, building setbacks, parking requirements, lot coverage by structures, and building heights. Historic preservation zoning overlay zoning does not prevent lot splits.

HP Zoning and the Tulsa Preservation Commission are governed by the City of Tulsa’s Zoning Code. Download the [Zoning Code](http://www.tulsapreservationcommission.org/zoning) at [www.tulsapreservationcommission.org/zoning](http://www.tulsapreservationcommission.org/zoning).



Before and after photos of a recent success story in the Brady Heights Historic District.

### **HISTORIC PRESERVATION PERMIT OVERVIEW**

The [Historic Preservation \(HP\) Permit](#) is the official document issued by the Tulsa Preservation Commission to approve proposed work to historic properties.

The TPC reviews the proposed work to make sure it complies with the design guidelines, in consideration of the needs and desires of the property owner. Once the TPC finds that the proposed work meets the design guidelines, a [Historic Preservation Permit](#) is issued, and the property owner may begin the work, or may apply for a building permit if one is required.

**Any exterior alterations or repair, new construction, or demolition within a historic preservation zoned neighborhood must be approved first by the Tulsa Preservation Commission or TPC Staff.**

Under certain conditions, TPC Staff can issue a [Historic Preservation Permit](#) without requiring a review by the Commission. There are also some exemptions to the [HP Permit](#) process, which Staff would be happy to discuss with you. To start the process of obtaining a [Historic Preservation Permit](#), contact TPC Staff at (918) 576-5687 or [preservationcommission@cityoftulsa.org](mailto:preservationcommission@cityoftulsa.org).

#### **Going Through the [HP Permit](#) Process**

The process of applying for and receiving a [Historic Preservation Permit](#) is very simple and straightforward with just a little advanced preparation. Consult TPC Staff early to ensure a smooth, successful application process — they are here to help!

Applicants complete a simple [application](#) and provide supporting photographs, brochures, and drawings as necessary. There is no fee for a [HP Permit](#) application.

#### **Steps for Property Owners**

1. Decide on what work you want to do on your property.
2. Refer to the district maps and confirm that your property is located in a historic preservation zoned district.
3. If your property is located in a HP zoning district, review the design guidelines for your neighborhood to determine the appropriate design and materials for the work you want to do. This is an ideal time to **contact TPC Staff**, who would be happy to assist you.
4. Develop a plan to carry out your work, including drawings of what you want your property to look like upon completion.
5. At this point, you are ready to submit a simple ~~one-page~~ application form describing the proposed work.
6. Your application will go through the [HP Permit](#) review process as explained below.

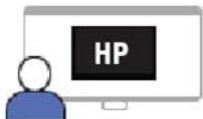
## UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES

The Historic Preservation Permit process is administered through public meetings. ~~The TPC meets twice monthly to approve COA applications: on the second Thursday of the month at 11:00 am and on the fourth Tuesday of the month at 4:30 pm.~~ The TPC meeting schedule is available from TPC staff or at [www.tulsapreservationcommission.org/meetings](http://www.tulsapreservationcommission.org/meetings). All meetings are held at City Hall. Applicants and interested parties are encouraged to attend.

A typical HP Permit application is presented at one TPC meeting. However, applications for new construction, ~~and~~ additions and complex projects are reviewed at two or more meetings because of the complex nature of those projects. The application is first presented to the HP Permit Subcommittee one week prior to the TPC meeting. New construction projects (brand new buildings on cleared land) are reviewed twice at the HP Permit Subcommittee level due to the complexity of the applications. After the HP Permit Subcommittee makes a recommendation on the application, it is reviewed by full Tulsa Preservation Commission, at which time a decision to approve or deny the application is made.



TPC Staff receives HP Permit Application along with supporting plans, photographs and brochures from applicant.



(*New construction, additions and complex projects*) At the HP Permit Subcommittee Meeting, TPC staff presents the application to the Subcommittee.



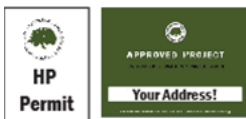
(*New construction, additions and complex projects*) HP Permit Subcommittee votes on a recommendation to the full Commission.



TPC staff presents the application to the full Tulsa Preservation Commission (*in the case of new construction, additions and complex projects, the HP Permit Subcommittee Chair explains the subcommittee's recommendation*).



The Tulsa Preservation Commission votes whether to approve or deny the application for a Historic Preservation Permit.



If approved, a Historic Preservation Permit is issued to the applicant, typically within a few business days. Applicants who are denied can reapply with an alternate proposal.

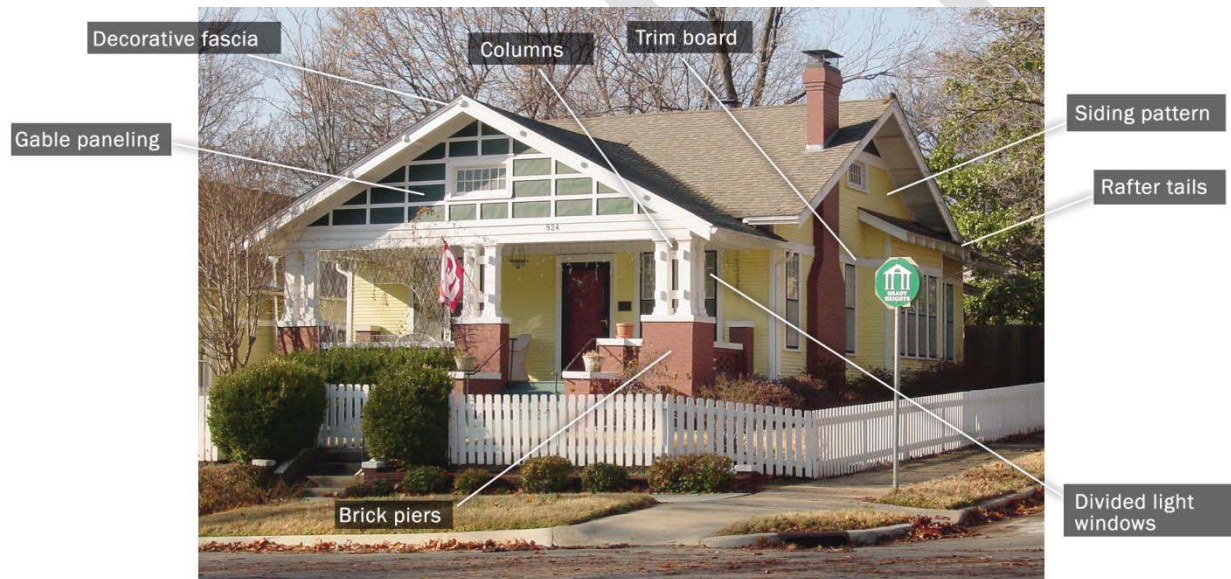
DRAFT

## **SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES**

### **A.1 General Requirements**

Use the following guidelines as the basis for all exterior work:

- A.1.1 Retain and preserve the existing historic architectural elements of your home.
- A.1.2 If replacement of historic architectural elements is necessary, match the size, shape, pattern, texture, and directional orientation of the original historic elements.
- A.1.3 Ensure that work is consistent with the architectural style and period details of your home.
- A.1.4 Return the structure to its original historic appearance using physical or pictorial evidence, rather than conjectural designs.



A.1.1 Examples of “historic architectural elements” to retain and preserve

### **Where to Find Physical Evidence**

Look closely the next time you are painting, removing artificial siding, or making repairs to the exterior of your home. If the historic fabric has been modified over the years, you can often find clues to what was there originally. Look for subtle changes in texture or material, or evidence of patching.

For example, many homes lost trim details when vinyl siding was added. After removing siding, you may notice areas of bare wood because a trim board, drip edge, or bracket was removed prior to re-siding. Do your soffits have unpainted stripes? You might be missing some eave brackets. On your porch, are there indentations on the piers or columns that tell you where a railing used to be attached?



## **SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES**

### **A.2 Exterior Walls**

- A.2.1 Retain and preserve the original historic wall materials and character-defining details, including, but not limited to, window and door trim, eave brackets, and embedded porch columns.
- A.2.2 To return the home to its original historic appearance, remove non-historic siding and trim. Repair and restore the underlying historic siding, trim, and details. Removal of non-historic siding is encouraged (and may be staff approved).
- A.2.3 If replacement of deteriorated wall materials is necessary, use matching materials (wood, brick, etc) that maintain the size, shape, pattern, texture, and directional orientation of the original historic material.
- A.2.4 When proposing to use materials different from the original historic wall materials, replacement materials that maintain the character of the structure and the size, shape, pattern, texture and directional orientation of the original historic siding will be considered on a case-by-case basis.
- A.2.5 In order to maintain the historic appearance of the structure, do not apply paint to unpainted brick or stone walls. Staff can approve the removal of paint from brick or stone surfaces to return the structure to its original historic appearance. Repainting previously painted surfaces does not require HP Permit review.

### **Common Types of Siding**



4" lap siding



2" Novelty lap siding



Shake shingles

### **How to Gently Remove Paint from Brick or Stone**

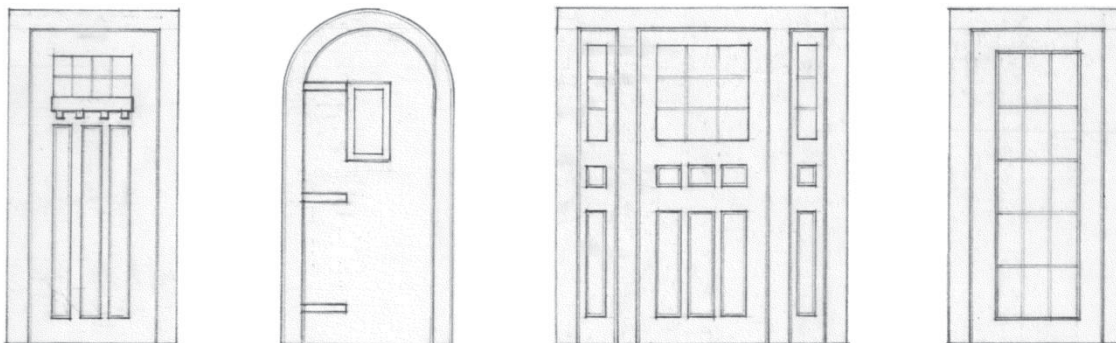
NEVER sandblast or power-wash masonry! Firing heavy artillery at an old building usually has disastrous results. Abrasive and acidic methods will remove the protective outer layer, causing brick to fail rapidly. Some chemicals can change the color of your masonry or allow harmful water infiltration. *Gel* and *paste* methods that gently dissolve paint are least likely to cause damage to your brick or the environment. These systems often involve applying paper to the paste and peeling it off.



## **SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES**

### **A.3 Doors and Door Surrounds**

- A.3.1 Retain and preserve original historic doors and door surrounds including frames, glazing, panels, sidelights, fanlights, and transoms.
- A.3.2 Do not remove, cover, or move existing door, sidelight, fanlight, and transom openings.
- A.3.3 To return the home to its original historic appearance, remove non-historic doors and replace them using physical or pictorial evidence of the originals. If no evidence exists, select doors and surrounds which are consistent with the architectural style of your home.
- A.3.4 To gain thermal efficiency, storm doors which maintain the appearance, and allow maximum visibility, of the original historic doors may be installed. Unfinished or clear-finished metals are not allowed. (Storm doors can be staff approved)
- A.3.5 If replacement of deteriorated doors is necessary, select doors and surrounds which are consistent with the architectural style of your home.
- A.3.6 If replacement of deteriorated trim is necessary, match the appearance, size, shape, pattern, texture, and detailing of the original historic trim.
- A.3.7 When adding new door openings, maintain the proportions of the façade. Match the dimensions and trim details of other doors and surrounds on your home. Select doors and surrounds which are consistent with the architectural style of your home.
- A.3.8 Use clear glass in new or replacement doors and sidelights.
- A.3.9 Exterior security bars and grills are discouraged.

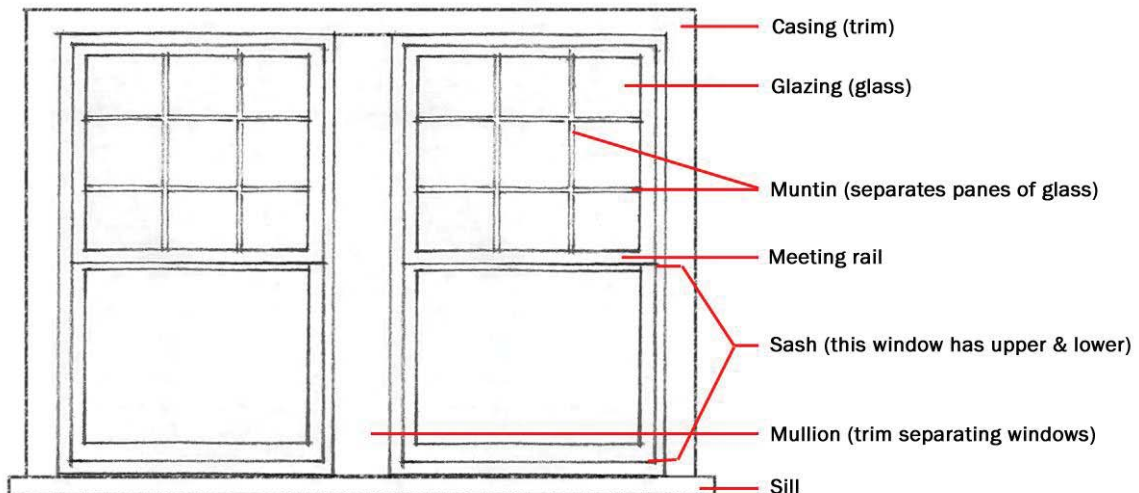


A.3.3 / A.3.5 Examples of door styles common to historic homes in Tulsa. From left to right, Craftsman door with dentil shelf, Tudor arched door, Colonial door with sidelights, 15-light French door appropriate for a variety of architectural styles.

## **SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES**

### **A.4 Windows and Window Trim**

- A.4.1 Retain and preserve original historic windows, including glazing, trim, muntins, and character-defining details.
- A.4.2 Do not remove, cover, or move existing window openings.
- A.4.3 To return the home to its original historic appearance, remove non-historic windows and trim. When selecting replacements, use physical or pictorial evidence. If no evidence exists, select windows which are consistent with the architectural style of your home.
- A.4.4 To gain thermal efficiency, storm windows which maintain the appearance, and allow maximum visibility, of the original historic windows may be installed. Unfinished and clear-finished metals are not allowed. (Storm windows can be staff approved)
- A.4.5 If replacement of deteriorated windows is necessary, match the original historic windows in sash design, size, shape, muntin pattern, location, glazing area and tint. Insulated glass (double pane) windows may be used. Exterior muntins are required on simulated divided light windows.
  - .1 Brady Heights – Match the original historic window material.
  - .2 Elmwood – Match the original historic window material
- A.4.6 If replacement of deteriorated trim is necessary, match the appearance, size, shape, pattern, texture, and detailing of the original historic trim.
- A.4.7 When adding new window openings, maintain the proportions of the façade. Match the size, design, and pattern of the existing windows. Align the headers of new windows with the existing windows.
- A.4.8 Exterior security bars and grills are discouraged.

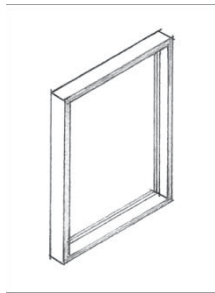


A.4.1 Parts of the window. With nine panes of glass in the upper sash and one pane in the lower sash, these windows are referred to as “nine over one”.

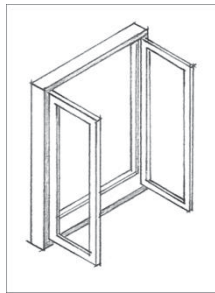
## SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES

### A.4 Windows and Window Trim

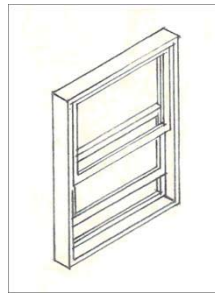
#### Types of Window Designs



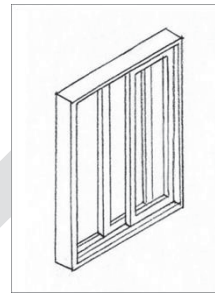
Fixed



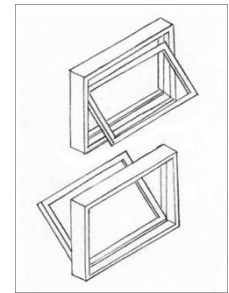
Casement



Double Hung



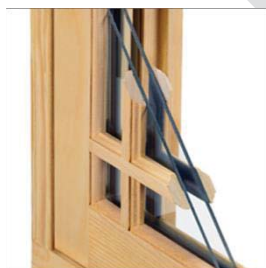
Sliding



Awning & Hopper



A.4.5 Inappropriate resizing, left window opening



A.4.5 Exterior muntins (left), called “simulated divided light” or SDL, create a profile that more closely resembles historic windows than grids between the panes of glass (right). True divided light windows are also available and encouraged.

#### Be Green: Restore Your Original Windows!

Restoration of your original windows not only costs less than replacing them, it's much more environmentally friendly.

If you're concerned about sustainability, don't throw your old-growth wood windows in the landfill before you explore restoration. Restoring your original windows not only maintains the architectural integrity and the curb appeal of your historic home, it's also **the best choice for the environment.**

Did you know that properly repaired historic windows have an R-value\* that's nearly indistinguishable from high-end replacement windows? Repair your original windows, install proper weatherstripping, and add a good storm window system – then watch your utility bills go down.

Contact Tulsa Preservation Commission Staff for information on how to make your historic windows look and function as good as new. We offer inexpensive, hands-on Wooden Window Repair workshops for DIY homeowners and contractors about once a year, budget permitting, and we can also refer you to other sources to help you treat your windows right!

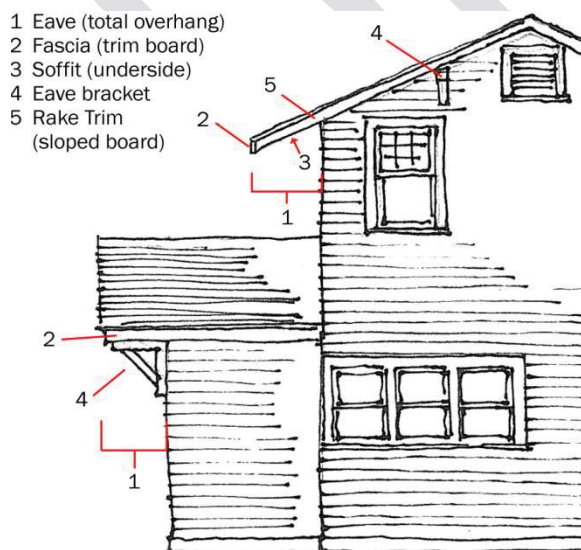
\*R-value is the measure of resistance to heat gain/loss

## SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES

### A.5 Roofs

- A.5.1 Retain and preserve the original historic roof form (hipped, gabled, etc.) and pitch.
- A.5.2 Do not remove character-defining architectural features of your roof including, but not limited to, dormers, chimneys, cupolas, eaves, soffits, fascia boards, and decorative details such as eave brackets, exposed rafter tails, or corbels.
- A.5.3 If replacement of deteriorated architectural roof features is necessary, use materials that maintain the character of the structure and the size, shape, pattern, texture, dimensions, and directional orientation of the original historic roof features.
  - .1 Elmwood – Match the original historic roof material
- A.5.4 To return the home to its original historic appearance, use physical or pictorial evidence. If no evidence exists, select architectural roof features which are consistent with the architectural style of your home.
- A.5.5 Replacement of existing roof covering – wood shingles, asphalt shingles, clay tile, etc. – with the same material does not require [HP Permit](#) review (for example, replacing an asphalt-shingled roof with asphalt shingles). Architectural shingles are encouraged.
- A.5.6 When proposing to change the materials of your roof covering, replacement materials that maintain the character of the structure and the size, shape, pattern, texture and directional orientation of the original historic roof covering will be considered on a case-by-case basis.
  - .1 Yorktown – Metal roofing is not allowed.
- A.5.7 When replacing your roof covering, replace an entire roof section if it is visible from the street.

### Parts of the Roofline



### Architectural Shingles

Architectural shingles, also referred to as dimensional or laminated shingles, are manufactured from fiberglass and asphalt and then laminated or textured.

This gives the shingles a more dimensional look than standard “three-tab” asphalt shingles.

The term “architectural shingles” can also refer to shingles that are made to simulate shake, slate, or other historic roof materials.

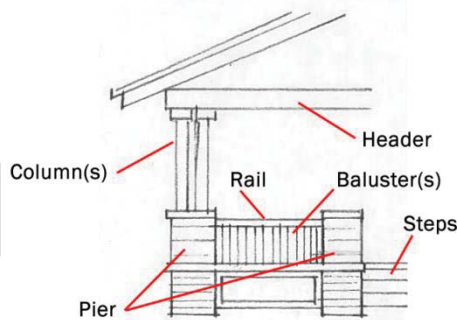


## **SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES**

### **A.6 Porches**

- A.6.1 Retain and preserve the original historic porch and its character-defining architectural features through repair.
- A.6.2 Do not remove character-defining architectural features of your porch including, but not limited to, ceiling, floor, piers, columns, railings, handrails, steps, bulkheads, skirt/stem wall, and decorative details such as crown molding, trim, eave brackets, and exposed rafter tails.
- A.6.3 If replacement of deteriorated porch elements is necessary, use materials that maintain the character of the structure and the size, shape, pattern, texture, dimensions, and directional orientation of the original historic features.
- A.6.4 To return the home to its original historic appearance, use physical or pictorial evidence. If no evidence exists, select porch features which are consistent with the architectural style of your home. Return enclosed porches to original historic open design (which can be staff approved).
- A.6.5 If adding a railing or other porch elements where none exists, select porch elements (columns, railing design, trim, etc.) which are consistent with the architectural style of your home.

#### **Parts of the Porch and Railing**



#### **Code Requirements**

Traditionally, historic railings in Tulsa are between 26 and 28 inches in height. The character of your home is affected when porch elements are altered from their historic proportions. When adding missing railings, be aware that porch elements must comply with building codes. TPC staff can help you navigate code requirements, such as determining whether historic building code provisions for existing structures apply.

#### **Good / Bad Examples of Railing Design**



Traditional design with railing lower than piers.



Decorative railing



Inappropriate design – balusters too far apart.

## SECTION A – GUIDELINES FOR REHABILITATION OF EXISTING STRUCTURES

### A.7 Awnings, Shutters, Mailboxes, Mechanical Systems, Etc.

- A.7.1 Select awnings that are consistent with the architectural style of your home.
- A.7.2 Select window shutters that are consistent with the architectural style of your home. Plastic shutters and door shutters are not historically accurate and are not allowed.
- A.7.3 Attach mailboxes to the front of the porch or house.
- A.7.4 Install engineering systems and their associated elements such as, but not limited to, air conditioning and heating units, package units, flues, conduits, cables, electrical boxes, ventilators, and louvers, on the side or rear façade of the structure.
- A.7.5 Install utility meters on the side or rear façade of the house, or underground in a subterranean vault.

A.7.6 Install systems requiring exterior components, such as solar panels or devices, where it they will have minimal impact, such as preferably on the side or rear of your house or yard or on an outbuilding. Install exterior components on a historic building in a manner that does not damage the historic roofing material or negatively impact the building's historic character and is reversible. These considerations will be made on a case-by-case basis. Install systems requiring exterior components, such as solar panels, on the side or rear of your house or in the back yard.

A.7.6A.7.7 Installation of satellite dishes does not require COA review. As determined by Staff, installation of radio or television antennas, including satellite dishes and similar devices, not visible from abutting streets, as determined by staff, are is exempt from HP Permit review.



A.7.2 This house style would not have originally had shutters. The shutters are inappropriate since they aren't less than half the width of the window, and so they do not appear operable. Also, dDoor shutters are also completely inappropriate for this house.



Even if your shutters are strictly decorative, they should appear operable. Mount your shutters with hinges directly on the window casing, never on the siding next to the window. Each shutter should be half the width of the window it surrounds.

## **SECTION B – GUIDELINES FOR ADDITIONS TO EXISTING STRUCTURES**

### **B.1 General Requirements**

Use the following guidelines as the basis for all additions:

- B.1.1 Locate additions on the side or rear of your home where the character-defining elements and visual appearance of the front façade will not be obscured, damaged, or destroyed.
- B.1.2 Ensure that additions do not detract from the historic appearance, character-defining elements, historic patterns, scale and proportions of the existing structure.
- B.1.3 Provide consistency and continuity between the addition and the historic portions of your home by using similar materials, style, forms, massing and scale.
- B.1.4 Do not exceed the established height of structures along the same street.



B.1.1 Locate additions on side or rear



B.1.1 Where NOT to locate an addition





## UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES

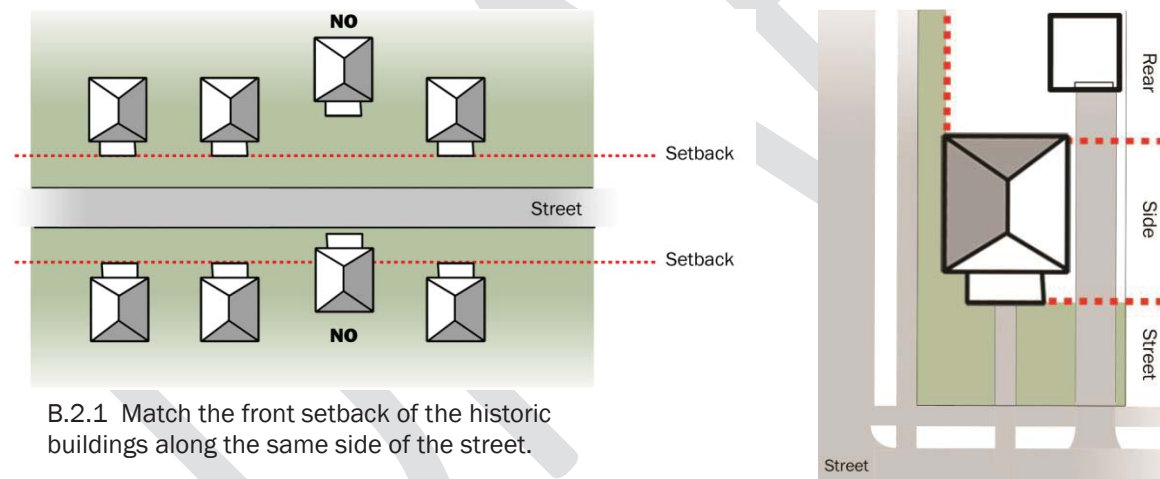
B.1.4 On the left, an example of an addition that doesn't respect the height of structures on same street. On the right, an example of an addition that matches the rhythm and character-defining features of the original structure. By offsetting or recessing additions behind the original building shows the addition is newer than the original structure.

DRAFT

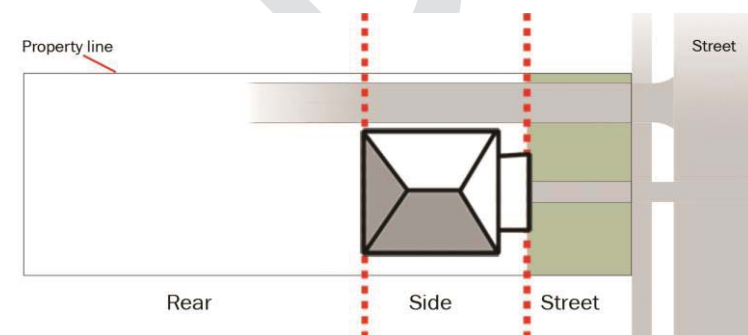
## SECTION B – GUIDELINES FOR ADDITIONS TO EXISTING STRUCTURES

### B.2 Building Site

- B.2.1 Match the front setback of the historic buildings along the same side of the street. When the front setback pattern of the historic structures on the same side of the street varies, locate the addition between the minimum and maximum of the prevailing front setbacks.
- B.2.2 Maintain the pattern and rhythm of the side yard setbacks of the other historic structures on the same side of the street.
- B.2.3 Limit paving within the street front yard to primary driveways and sidewalks. Curb cuts and new driveways through the front-street yard are strongly discouraged for properties with alley access.
- B.2.4 On interior lots, limit the surface area of driveways and sidewalks to no more than 50% of the front-street yard lot area.  
.1 Elmwood – no more than 25% of front-street yard lot area
- B.2.5 On corner lots, limit the surface area of driveways and sidewalks to no more than 30% of the combined front-and-side-street yard lot area.



B.2.1 Match the front setback of the historic buildings along the same side of the street.



B.2.3 – B.2.5 On the left, defining the front-street yard lot area (green) of-on interior lots. Above, defining the combined front-and-side-street yard lot area (green) on corner lots. The surface area of paving within these defined areas must not exceed the percentages listed above.

## **SECTION B – GUIDELINES FOR ADDITIONS TO EXISTING STRUCTURES**

### **B.3 Building Materials and Elements**

- B.3.1 Use building materials that are consistent with the historic materials found on the existing structure.
- B.3.2 The use of building materials different from historic materials will be considered on a case-by-case basis, if the proposed materials maintain the character of the structure and the historic size, shape, pattern, texture and directional orientation. Unfinished or clear-finished metals are not allowed.
- B.3.3 Create a visual consistency and continuity with the building elements of the existing structure by replicating the height of windows and doors, window glazing patterns, roof forms, eave lines and overhangs, and special detailing present on the existing structure.

## **SECTION B – GUIDELINES FOR ADDITIONS TO EXISTING STRUCTURES**

### **B.4 Roofs**

- B.4.1 New roof features, such as dormers or cupolas, may be added to the existing roof, if the proposed elements maintain the scale, proportions, rhythms, and architectural character of the structure.
- B.4.2 On additions, use roof forms, slope, detailing, and roofing materials that are consistent with the historic portions of the existing structure. [Installation of metal roofs will be considered on a case-by-case basis.](#)
  - .1 Yorktown – Metal roofing is not allowed.

## **SECTION B – GUIDELINES FOR ADDITIONS TO EXISTING STRUCTURES**

### **B.5 Porches**

- B.5.1 When adding a porch or expanding an existing porch, maintain the character, detailing, scale, rhythm, and proportions of the existing historic structure.
- B.5.2 When enclosing porches with glass or screens, maintain the existing rhythms, scale, proportions, appearance, and character of the structure. Hard wall enclosure of porches is strongly discouraged but will be considered on a case-by-case basis.
- B.5.3 Ramps may be installed when required to achieve access to the first floor level. Construct the ramp so that, in the future, it can be removed without significantly altering the original historic structure. Designing ramps to access porches from the side, so the front elevation is not obscured, is encouraged.



B.5.2 Example of enclosing a front porch with screens that maintains the character of the Prairie style house and can be removed in the future without damaging the historic brick columns.



B.5.3 Example of an [accessibility](#) ramp that enters the front porch from the side. The ramp can be easily removed in the future without needing to repair the historic materials on the front elevation.

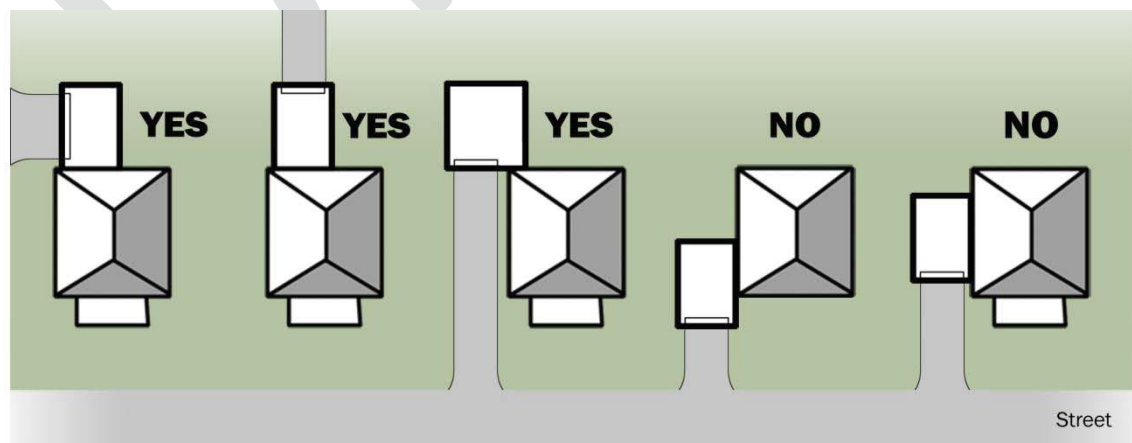
### **Enclosing Front Porches**

Enclosing the front porch is discouraged because it usually compromises the architectural integrity of the home. This is especially true of hard wall enclosures. If you wish to enclose your front porch, it's important that you do so in a way that doesn't detract from the historic appearance of the home and can be easily removed later without damaging the historic porch materials. Screens and clear glass with minimal framing are the best options.

## **SECTION B – GUIDELINES FOR ADDITIONS TO EXISTING STRUCTURES**

### **B.6 Garages**

- B.6.1 Locate garages within the rear yard and detached from the primary residential structure. ~~Detached garages located in the rear yard are exempt from COA review. Detached buildings or structures, such as garages and sheds, not located in the street yard, as defined in the Zoning Code, are exempt from HP Permit review.~~
- B.6.2 Adding a garage attached to the rear elevation of the primary residential structure will be considered on a case-by-case basis. Locate attached garages so that the front façade of the garage is not located forward of the rear wall of the primary structure.
- B.6.3 Historically appropriate garage doors, such as carriage house doors, are encouraged.



B.6.2 Illustration of where an attached garage may be located (reviewed on a case-by-case basis).

## **SECTION B – GUIDELINES FOR ADDITIONS TO EXISTING STRUCTURES**

### **B.7 Mechanical Systems, Etc.**

- B.7.1 Install engineering systems and their associated elements such as, but not limited to, air conditioning and heating units, flues, conduits, cables, electrical boxes, ventilators, and louvers, on the side or rear façade of the structure.
- B.7.2 Install utility meters on the side or rear façade of the house, or underground in a subterranean vault.

B.7.3 Install systems requiring exterior components, such as solar panels or devices, where they will have minimal impact, such as preferably on the side or rear of your house or yard or on an outbuilding. Install exterior components on a building in a manner that does not damage the roofing material or negatively impact the building's historic character and is reversible. These considerations will be made on a case-by-case basis. Install systems requiring exterior components, such as solar panels, on the side or rear of your house or in the back yard.

B.7.4 Installation of satellite dishes does not require COA review. As determined by Staff, installation of radio or television antennas, including satellite dishes and similar devices, not visible from abutting streets, as determined by staff, are-is exempt from HP Permit review.



## **SECTION C – GUIDELINES FOR NEW CONSTRUCTION**

### **C.1 General Requirements**

- C.1.1 Designs for new construction shall not duplicate existing structures within the district. Ensure that each new structure is unique within the district.
- C.1.2 When designing new structures, provide consistency and continuity by drawing upon common characteristics of historic structures in the district, placing particular emphasis on the historic structures on the same street. These include but are not limited to porches, entries, roof pitch and form, and window and door styles.
- C.1.3 Avoid mixing incongruous architectural styles: for example, Prairie-style windows on a Colonial Revival-inspired house.
- C.1.4 Respect the scale, proportions, historic patterns, and relationships of both principal and accessory structures along the same street and within the district.
- C.1.5 Maintain the established height of those structures along the same side of the street.

~~C.1.5~~ C.1.6 ~~Maintain the established~~ Establish the height of the floor (finished floor elevation) between the minimum and maximum finished floor elevation of those structures along the same side of the street.



C.1.1 / C.1.2 While not a duplicate of existing houses, this house draws upon common characteristics of the district – raised crawl space, porch design, multiple front gables, siding patterns and grouped windows.



C.1.4 The detached garage (accessory structure) at right relates to the house (principal structure) in scale and pattern in similar fashion to the historic houses in the district.

## UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES

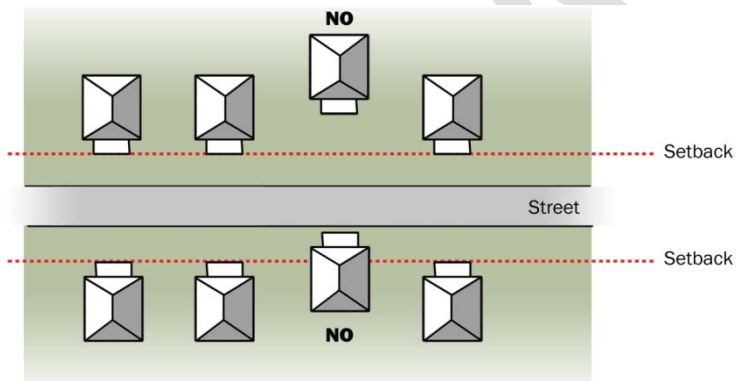


C.1.5 Example of infill that doesn't respect the height of structures on same street.

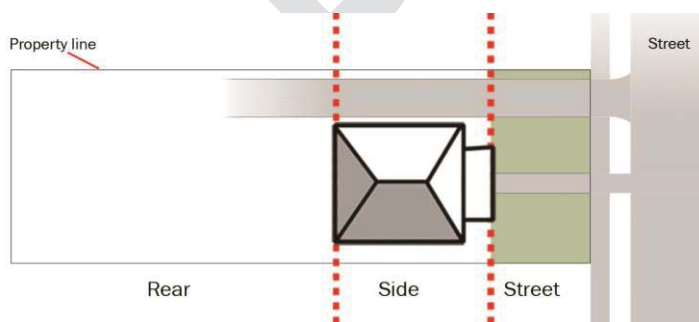
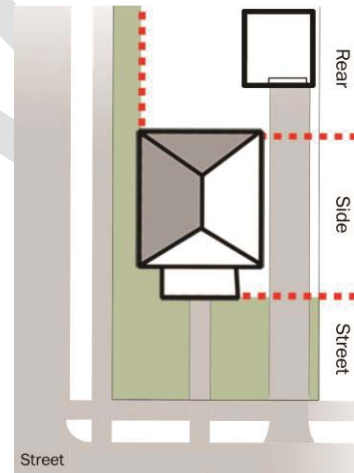
## SECTION C – GUIDELINES FOR NEW CONSTRUCTION

### C.2 Building Site

- C.2.1 Match the front setback of the historic buildings along the same side of the street. When the front setback pattern of the historic structures on the same side of the street varies, locate the new structure between the minimum and maximum of the prevailing front setbacks.
- C.2.2 Maintain the pattern and rhythm of the side yard setbacks of the other historic structures on the same side of the street.
- C.2.3 Maintain the same orientation to the street as established by the historic structures on the same street.
- C.2.4 Limit paving within the street front yard to primary driveways and sidewalks. Curb cuts and new driveways through the front street yard are strongly discouraged for properties with alley access.
- C.2.5 On interior lots, limit the surface area of driveways and sidewalks to no more than 50% of the front street yard lot area.
- C.2.6 On corner lots, limit the surface area of driveways and sidewalks to no more than 30% of the combined front and sidestreet yard lot area.



C.2.1 Match the front setback of the historic buildings along the same side of the street.



C.2.4 – C.2.6 On the left, defining the street yard lot area (green) of-on interior lots. Above, defining the combined street and side yard lot area (green) on corner lots. The surface area of paving within these defined areas must not exceed the percentages listed above.

## **SECTION C – GUIDELINES FOR NEW CONSTRUCTION**

### **C.3 Building Materials**

**C.3.1** Maintain the visual characteristics, scale, proportions, directional orientation and rhythms that are created by the materials on existing historic structures in the district, in order to maintain the overall appearance and character of the district. Deviation from the materials on existing historic structures in the district will be considered on a case-by-case basis. The use of Unfinished or clear-finished metals are not allowed will be considered on a case-by-case basis.

## **SECTION C – GUIDELINES FOR NEW CONSTRUCTION**

### **C.4 Garages**

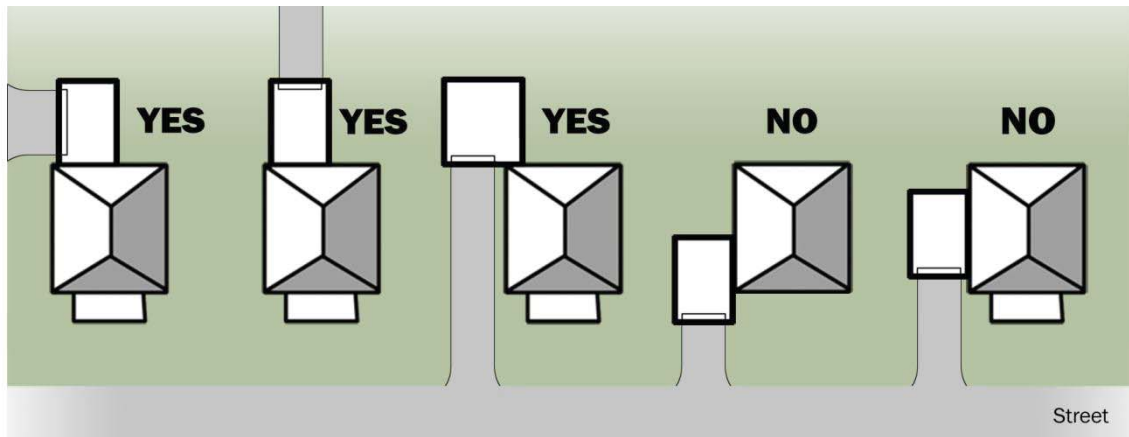
C.4.1 Locate garages within the rear yard and detached from the primary residential structure. ~~Detached garages located in the rear yard are exempt from COA review. Detached buildings or structures, such as garages and sheds, not located in the street yard as defined in the Zoning Code are exempt from HP Permit review.~~

C.4.2 Adding a garage attached to the rear elevation of the primary residential structure will be considered on a case-by-case basis. ~~Locate attached garages so that the front façade of the garage is not located forward of the rear wall of the primary structure.~~

~~C.4.2~~ C.4.3 Permitted attached garages shall be located so that the front façade of the garage is not located forward of the rear wall of the primary structure.

~~C.4.3~~ C.4.4 Historically appropriate garage doors, such as carriage house doors, are encouraged.

## UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES



C.4.2 Illustration of where an attached garage may be located.

## **SECTION C – GUIDELINES FOR NEW CONSTRUCTION**

### **C.5 Mechanical Systems, Etc.**

C.5.1 Install engineering systems and their associated elements, such as, but not limited to, air conditioning and heating units, flues, conduits, cables, electrical ~~boxes~~ equipment, ventilators, and louvers, on the side or rear façade of the structure.

C.5.2 Install utility meters on the side or rear façade of the house or underground in a subterranean vault. Above-ground installation of utility systems, such as pedestals and transformers, is prohibited in the street yard unless approved on a case-by-case basis.

C.5.3 Install systems requiring exterior components, such as solar panels or devices, where it they will have minimal impact, such as preferably on the side or rear of your house or yard or on an outbuilding. Install exterior components on a building in a manner that does not damage the roofing material or negatively impact the building's historic character and is reversible. These considerations will be made on a case-by-case basis. Install systems requiring exterior components, such as solar panels, on the side or rear of your house or in the back yard.

~~C.5.3~~C.5.4 Installation of satellite dishes does not require COA review. As determined by Staff, installation of radio or television antennas, including satellite dishes and similar devices, not visible from abutting streets, as determined by staff, are-is exempt from HP Permit review.



## **SECTION D – GUIDELINES FOR RELOCATED STRUCTURES**

### **D.1 General Requirements**

D.1.1 Maintain the architectural integrity of the district for the period of time when each addition was originally platted when relocating a structure.

D.1.2 Respect the scale, proportions, historic patterns, and relationships of primary to accessory structures within the district when relocating a structure.

**D.1.3** Maintain the established height of structures along the same street when relocating a structure into the district.

~~D.1.3~~ **D.1.4** Maintain the established height of the exposed foundation system as structures along the same street Establish the height of the floor (finished floor elevation) between the minimum and maximum finished floor elevation of those structures along the same side of the street.

~~D.1.4~~ **D.1.5** Upon placement of the structure on the new site, follow Section A (Rehabilitation) and Section B (Additions) when replacing materials and elements that were removed to facilitate relocation.



## **UNIFIED DESIGN GUIDELINES – RESIDENTIAL STRUCTURES**

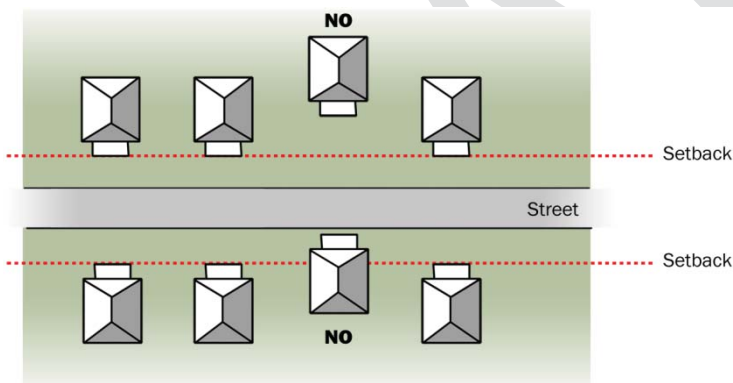
D.1.3 Example of infill that doesn't respect the height of structures on same street.

DRAFT

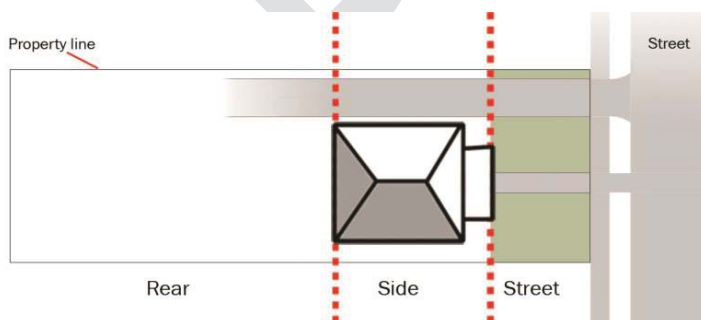
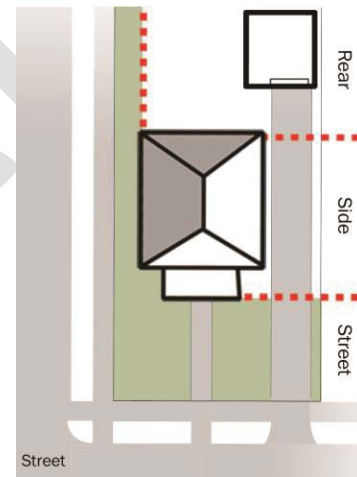
## SECTION D – GUIDELINES FOR RELOCATED STRUCTURES

### D.2 Building Site and Placement

- D.2.1 Match the front setback of the historic buildings along the same side of the street. When the front setback pattern of the historic structures on the same side of the street varies, locate the structure between the minimum and maximum of the prevailing front setbacks.
- D.2.2 Maintain the pattern and rhythm of the side yard setbacks of the other historic structures on the same side of the street.
- D.2.3 Maintain the same orientation to the street as established by the historic structures on the same street.
- D.2.4 Limit paving within the streetfront yard to primary driveways and sidewalks. Curb cuts and new driveways through the streetfront yard are strongly discouraged for properties with alley access.
- D.2.5 On interior lots, limit the surface area of driveways and sidewalks to no more than 50% of the front-street yard lot area.
- D.2.6 On corner lots, limit the surface area of driveways and sidewalks to no more than 30% of the combined front-street and side yard lot area.



D.2.1 Match the front setback of the historic buildings along the same side of the street.



D.2.4 – D.2.6 On the right, defining the front-street yard lot area (green) ~~of-on~~ interior lots. On the left, defining the combined front and side yard lot area (green) of corner lots. The surface area of paving within these defined areas must not exceed the percentages listed above.

## **SECTION D – GUIDELINES FOR RELOCATED STRUCTURES**

### **D.3 Mechanical Systems, Etc.**

- D.3.1 Install engineering systems and their associated elements such as, but not limited to, air conditioning and heating units, flues, conduits, cables, electrical boxes, ventilators, and louvers, on the side or rear façade of the structure.
- D.3.2 Install utility meters on the side or rear façade of the house, or underground in a subterranean vault.

D.3.3 Install systems requiring exterior components, such as solar panels or devices, where they will have minimal impact, such as preferably on the side or rear of your house or yard or on an outbuilding. Install exterior components on a building in a manner that does not damage the roofing material or negatively impact the building's historic character and is reversible. These considerations will be made on a case-by-case basis. Install systems requiring exterior components, such as solar panels, on the side or rear of your house or in the back yard.

D.3.3D.3.4 Installation of satellite dishes does not require COA review. As determined by Staff, installation of radio or television antennas, including satellite dishes and similar devices, not visible from abutting streets, as determined by staff, are-is exempt from HP Permit review.

## **SECTION E – GUIDELINES FOR NON-CONTRIBUTING STRUCTURES**

### **E.1 General Requirements**

- E.1.1 For the purposes of this chapter, non-contributing structures are those listed as not contributing to the historic character of the district due to age or architectural style in the National Register Nomination for the district.
- E.1.2 Non-contributing structures will be considered products of their own time. Do not attempt to create a false appearance of the predominant character and architectural style of the rest of the district.
- E.1.3 Follow Section A (Rehabilitation) and Section B (Additions) as they relate to the character-defining elements of the non-contributing structure.
- E.1.4 Ensure that work on non-contributing structures does not detract from or diminish the historic character of the overall district.

## **SECTION F – GUIDELINES FOR DEMOLITION OF STRUCTURES**

### **F.1 General Requirements**

- F.1.1 Demolition of historic structures does not support the character of the district. Demolitions are strongly discouraged, but will be considered on a case-by-case basis provided that one or more of the following guidelines are met:
- .1 The continued use of the property cannot be achieved through rehabilitation, modification, or alteration, according to documented evidence.
  - .2 The structure is listed as non-contributing to the historic character of the district in the National Register Nomination for the district.
  - .3 To remedy an emergency condition determined to be dangerous to life, health, or property, or if the structure has been condemned by the City of Tulsa or City-County Health Department.

#### **Assessing Rehabilitation Feasibility**

Professionals from the American Institute of Architects, Eastern Oklahoma Chapter, can help property owners assess rehabilitation options on a pro bono or reduced fee basis. Ask TPC staff for more information on these services.



## SECTION G – GUIDELINES FOR LANDSCAPE FEATURES, PAVING AND SIGNAGE

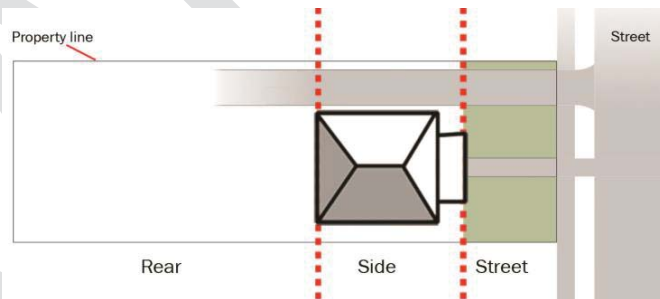
### G.1 Landscape Features

- G.1.1 Retain and preserve original historic walls, fencing, lighting, planters, and other landscape features through repair.
- G.1.2 Removal of historic landscape features will be considered on a case-by-base basis. Removal of non-historic landscape features can be staff-approved.
- G.1.3 Ensure that new landscape features are appropriate to the style of your home and consistent with the historic elements found along the same street and within the district.
- G.1.4 Use fencing materials that are consistent with the historic fencing found along the same street and within the district. Chain link fencing, wire fencing (12 gauge or less), vinyl fencing, or any fencing that blocks the view of structures is not allowed.
- G.1.5 Use wall materials that are consistent with the historic walls found along the same street and within the district. Cinder block, segmental retaining wall systems, corrugated metal, and railroad ties are not allowed. Historically styled cast concrete block will be considered on a case-by-case basis.

.1 Elmwood – dry-stack retaining walls are not allowed

#### Trees, Shrubs, Flowers

Planting organic materials such as trees, grasses, shrubs, and flowers are exempt from review.



Landscape features are reviewed only in the street yard (green). Fences, walls, and paving in the side or rear back yard are exempt from review.



G.1.4 Low wooden picket (and, in some instances, wrought iron) fences are most appropriate.



G.1.5 Historically, retaining walls were constructed of native stone or brick.

## **SECTION G – GUIDELINES FOR LANDSCAPE FEATURES, PAVING AND SIGNAGE**

### **G.2 Paving**

- G.2.1 Retain and preserve original historic paving, steps, and bulkheads through repair.
- G.2.2 Ensure that the design of new paving is consistent with historic elements found along the same street and within the same neighborhood.
- G.2.3 Use paving materials that are consistent with the historic paving found along the same street and within the same neighborhood. Asphalt and stained concrete are not allowed.
- G.2.4 Surface parking lots do not support the historic character of the district. Construction of parking lots is strongly discouraged, but will be considered on a case-by-case basis provided that the following guidelines are met:
  - .1 Match the front setback pattern of the historic structures on the same side of the street. On corner lots, match the setback pattern of the historic structures on all adjoining streets. When the setback pattern of the historic structures on the same side of the street varies, locate the parking lot between the minimum and maximum of the prevailing setbacks.
  - .2 The screening that is required by the Zoning Code shall meet the conditions of G.1.4 and G.1.5. Ensure that screening is of sufficient height and density to obscure the view of the parking lot from the street and adjacent historic structures.
  - .3 When possible, provide vehicular access to the parking lot from an alley or arterial street to minimize the traffic impact on residential streets.



G.2.1 Bulkheads are the sloped strips of paving flanking the steps above. Note the width of the paving and how the angle is flush with the sidewalk, not raised above it.



G.2.2 Historically, driveways in Tulsa were plain concrete slabs. A great alternative is the ribbon driveway (above), which uses less concrete, creates less runoff, and is historically appropriate.

## **SECTION G – GUIDELINES FOR LANDSCAPE FEATURES, PAVING AND SIGNAGE**

### **G.3 Signage**

- G.3.1 Ensure that signage is consistent in design with the historic character of the neighborhood. Construct signs of materials similar to those used for existing historic structures in the neighborhood, such as wood, stone or brick.